

— City of —  
PICKERING

# Urban Design Guidelines

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for

# Infill & Replacement Housing in Established Neighbourhood Precincts

Prepared by

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**SCL**

August  
2020





SCL



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**The City's distinctive landscape, history, location and settlement pattern are valuable assets. Properly nurtured, these assets can set Pickering apart from other municipalities. The City's unique identity must be fostered and promoted.**



**City of Pickering,  
Official Plan**

# 1

## Introduction

### 1.1 Why Were These Urban Design Guidelines Developed?

Throughout several of the City of Pickering's established neighbourhoods, houses are being replaced with larger houses, existing houses are being renovated and new houses are being built. Faced with these development pressures, the City undertook the Infill and Replacement Housing in Established Neighbourhoods Study (IRHEN Study) to address a few key objectives:

1. To identify the City's established neighbourhoods, or parts thereof, within the South Pickering Urban Area that may be susceptible to pressure for the development of infill and replacement housing;
2. To identify and evaluate the unique qualities and characteristics of the City's established neighbourhoods, or parts thereof, and the key issues regarding infill and replacement housing that are of concern to residents;
3. To identify and/or develop tools the City can use, including Design Guidelines, that will allow neighbourhoods, or parts thereof, to evolve while respecting the character of the area; and

4. To provide an opportunity for full and meaningful engagement and consultation with residents, agencies and the development industry through the study process.

These Urban Design Guidelines (Guidelines) were developed as part of the recommendations from the IRHEN Study prepared by SGL Planning and Design Inc. The IRHEN Study developed options, including making recommendations for an appropriate policy framework, regulations and/or tools that may be implemented so that the City has a sensitive way to manage new construction in established residential neighbourhoods.

### Urban Design Guidelines



Help to refine the sense of character of a place through Design Principles;

Provide detailed design direction to help implement a municipality's vision of a particular area or neighbourhood;

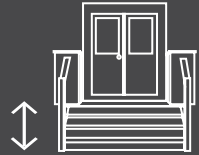
Help implement policies in the official plan and provisions in the zoning by-law; and

Are used by staff, developers and the public for evaluation and preparation of development or re-development applications.

The Urban Design Guidelines have been developed to support and enhance the following neighbourhood characteristics:



Dwelling Height and Roof Pitch



Height of the front entrance



Dwelling length, width and depth



Side yard setback and separation distance between dwellings



Garage or carport placement



Driveway width



Front yard landscaping

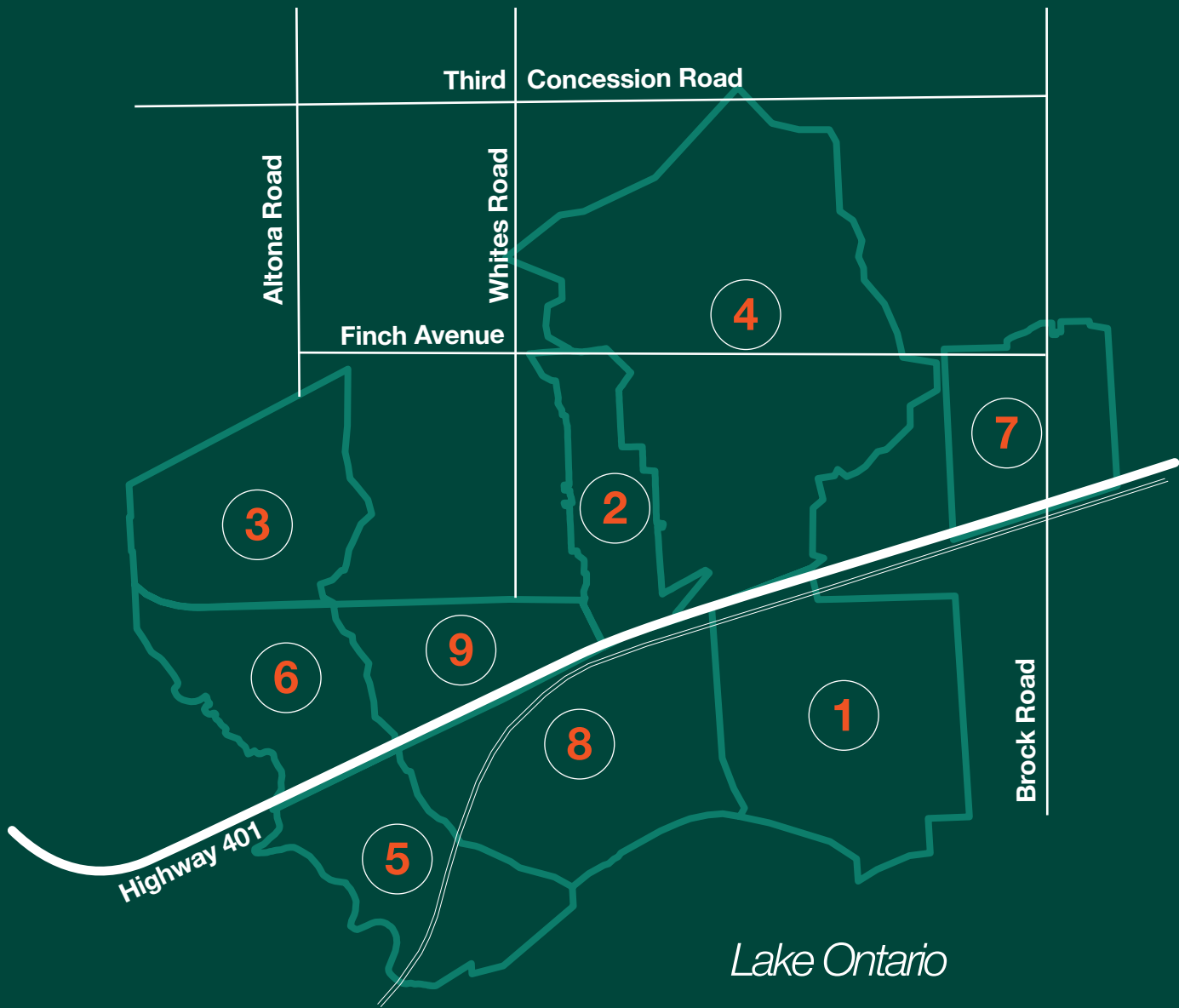


Street trees

## 1.2 Where do these Guidelines Apply?

Within the South Pickering Urban Area there are fifteen neighbourhoods. Of these fifteen neighbourhoods, the IRHEN Study identified nine Focus Neighbourhoods (Figure 1) where most of the infill and replacement housing has been constructed over the last few years.

These guidelines apply to the Neighbourhood Precincts within the nine Focus Neighbourhoods as delineated within the IRHEN Study (Figure 2). The detailed delineated boundaries of the Neighbourhood Precincts are found in Appendix B.



### FOCUS NEIGHBOURHOODS

- |               |                    |
|---------------|--------------------|
| 1. Bay Ridges | 6. Rougemount      |
| 2. Dunbarton  | 7. Village East    |
| 3. Highbush   | 8. West Shore; and |
| 4. Liverpool  | 9. Woodlands       |
| 5. Rosebank   |                    |

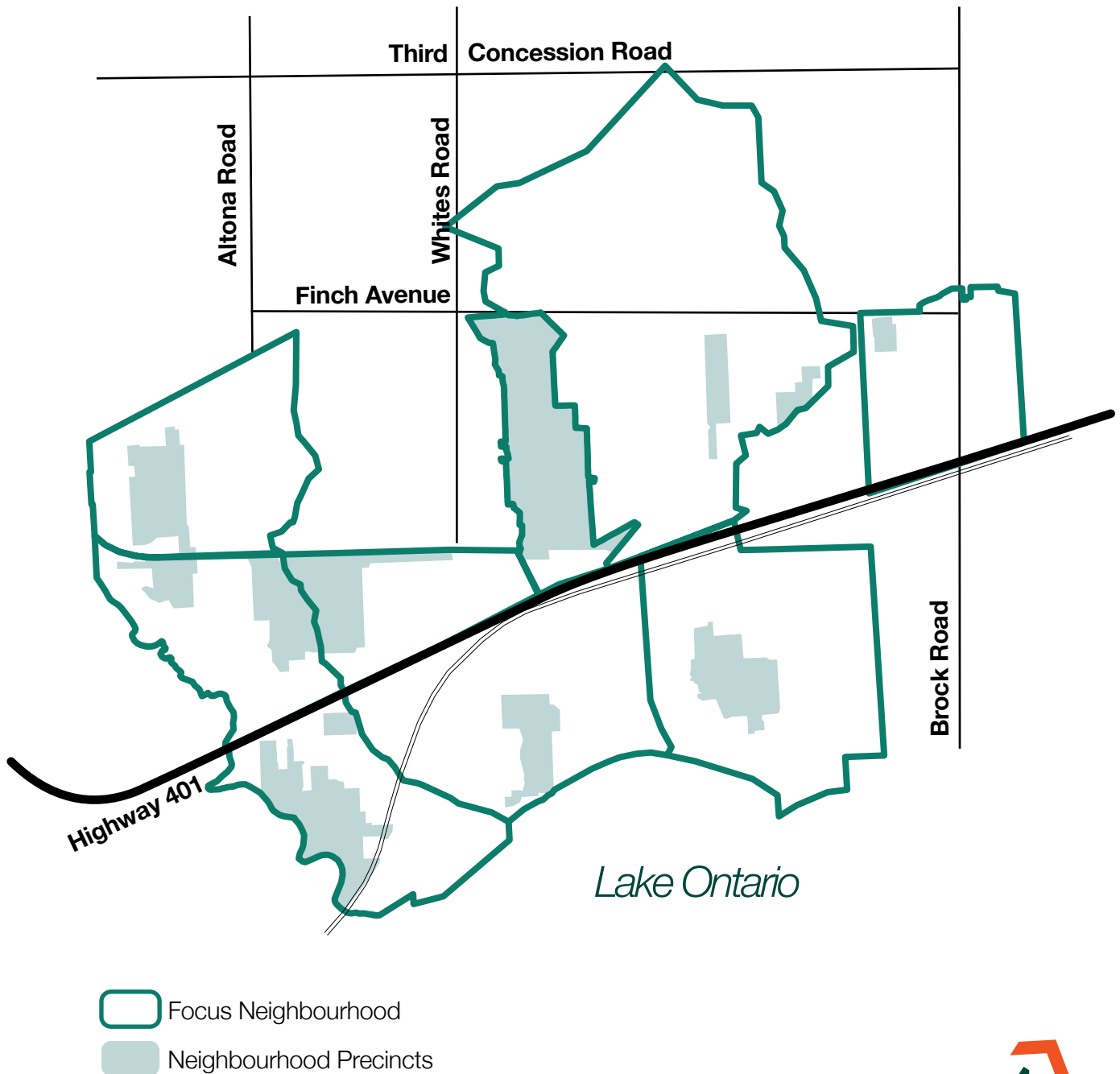


Figure 1: City of Pickering Focus Neighbourhoods



## Neighbourhood Precincts

These Neighbourhood Precincts are areas within a Focus Neighbourhood that have been delineated based on a combination of age of dwellings, existing lot coverage, and where there have been many observed changes related to infill and replacement housing



**FIGURE 2:** Map of Neighbourhood Precincts within Focus Neighbourhoods



## These Guidelines Use the Following Definitions:

### Infill

Means the development of two or more ground-oriented housing forms such as single detached, semi-detached and townhouse dwellings through a consolidation of lots or the severance of a larger lot. Within the context of this document, infill does not include the development of stacked townhomes, stacked back-to-back townhomes, apartment buildings, or other forms of multi-unit housing resulting in a higher level of density. However, such proposals may be appropriate and would be evaluated on a site-specific basis. Infill can occur through draft plan of subdivision/condominium, site plan and/or land division on a site-specific basis.

### Replacement Housing

Involves a smaller dwelling being substantially altered or demolished and replaced with a new, larger dwelling through the building permit application process and possibly the minor variance process.

#### Note:

Existing housing stock will not be affected by these guidelines unless subject to a development application.



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## 1.3 Vision

The vision for the City of Pickering's Neighbourhood Precincts within the nine Focus Neighbourhoods is to support and enhance the character elements that collectively create a sense of place, identity and enjoyment.

Recognizing that change is occurring within the Neighbourhood Precincts, the Guidelines provide a better way to manage new and infill development to support the existing character rather than freezing neighbourhoods in time.



**Figure 3:** Images of infill and replacement housing within Pickering's Focus Neighbourhoods that respects existing character through design elements ranging from height, garage placement, setback, landscaping, roof slope, and tree protection.



## 1.4 Objectives & Principles

### Objectives

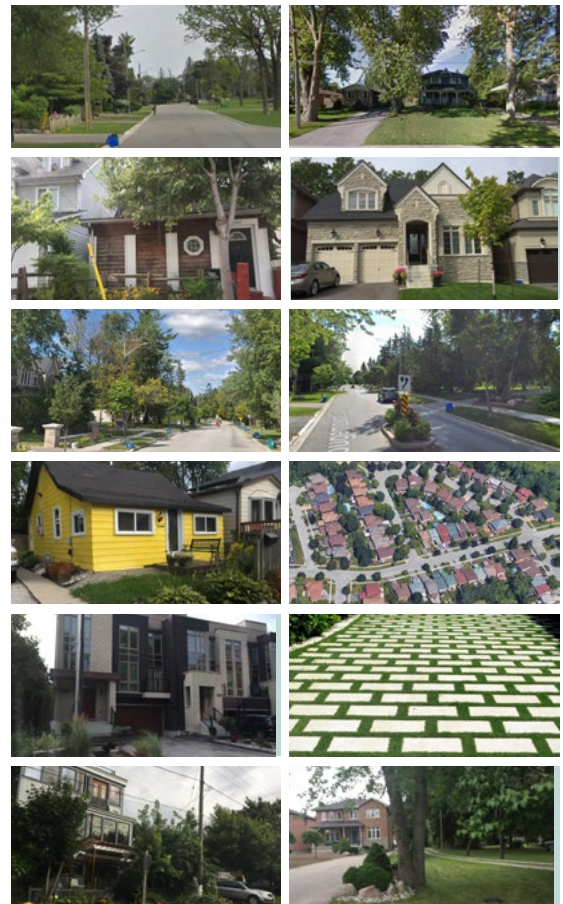
The objectives of the Urban Design Guidelines are informed by the City's Official Plan Community Design goals and policies and the Official Plan's Detailed Design Considerations, and support the intent of the IRHEN Study.

The Guidelines have been developed to address design goals that include human scale, pedestrian comfort, permeability, context, legibility and natural heritage, as well as detailed design consideration for community image, development and subdivision design.

### Principles

These Urban Design Guidelines intend to provide guidance and serve as an example of key principles and policies supported by the City of Pickering's Official Plan. These Guidelines are based on the following design principles:

- 1 Enhance and integrate new built form that is compatible with the characteristics of the Neighbourhood Precinct.
- 2 Encourage architectural diversity that complements the character of the Neighbourhood Precinct.
- 3 Encourage pedestrian friendly neighbourhoods that foster a healthy and inclusive community.
- 4 Enhance the character, identity and sense of place of the Neighbourhood Precinct.
- 5 Incorporate low impact design opportunities to mitigate the potential increase in stormwater runoff related to an increase in impervious surface and to enhance the ecosystem health.
- 6 Encourage the greening of streetscapes and private property.



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## 1.5 How to Use the Document

Urban Design Guidelines are used to coordinate the various components of built form, streetscape and neighbourhood composition to create an attractive, appealing and functional urban environment. These Guidelines are one of the useful tools identified from the IRHEN Study to help ensure both a high quality public and private realm within the Neighbourhood Precincts.

Urban Design Guidelines establish baseline standards to guide the development of infill and replacement housing, providing for greater design rationale and detail.

These Guidelines should be included in the Compendium Document to the Official Plan and are to be used in conjunction with the Official Plan, existing Development Guidelines, zoning by-laws and other planning tools.

The Compendium Document of the City's Official Plan includes development guidelines for certain neighbourhoods, or parts of certain neighbourhoods. There are three development guidelines that apply in the context of the Neighbourhood Precinct that must be read in conjunction with these Guidelines:

- Dunbarton Neighbourhood: Dunbarton Neighbourhood Development Guidelines;
- Bay Ridges Neighbourhood: Liverpool Road Waterfront Node Development Guidelines; and
- Rosebank Neighbourhood: Rosebank Neighbourhood Development Guidelines.

Each of these development guidelines provides a differing level of detail.

The Dunbarton Neighbourhood Development Guidelines contains area specific guidelines within the neighbourhood, addressing matters such as permitted dwelling type, minimum lot frontage, minimum front and side yard setbacks, maximum building height, and various guidelines for the public realm including the provision of sidewalks and cycle paths, and connectivity to natural areas. The Liverpool Road Waterfront Node Development Guidelines address strategies for open space, development standards to implement an effective transportation network, preserving views and vistas, providing for adequate parking areas, and promoting appropriate built form in keeping with the Great Lakes Nautical Village 'vision' for the neighbourhood.

The Rosebank Neighbourhood Development Guidelines establish guidelines for certain precincts within the neighbourhood, that address limited matters including permitted dwelling types, minimum lot size, and lot frontage.

To aid in the application of these Guidelines a checklist is provided at the end of the document (Appendix A) that summarizes the key intentions of the guidelines.

Urban Design Guidelines should be read in conjunction with the Development Guidelines, and in the case of a conflict the Urban Design Guidelines take precedence.





**We shape our buildings;  
thereafter they shape us.**



**Winston Churchill**

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# 2

## Built Form

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Built Form examines the design elements of building type and architectural form that contribute to the massing and appearance of buildings.

The compatibility issues currently existing within the City of Pickering's Neighbourhood Precincts (Figure 4) as they relate to the elements of Built Form result from new housing typically being taller and larger than the original homes on the street.

New homes can have a higher elevation of the first floor, greater lot coverage, reduced separation distances between neighbouring dwellings and reduced landscaped open area in the front yards.

The overall scale, massing and placement of the new homes can create an obvious contrast when located beside more modestly sized original homes. This contrast can sometimes lead to impacts such as shadowing, overlook, and privacy.

This section provides guidelines to help address compatibility issues through these Built Form design elements:

- Dwelling height and roof pitch;
- Height of front entrance; and
- Dwelling length, width and depth.

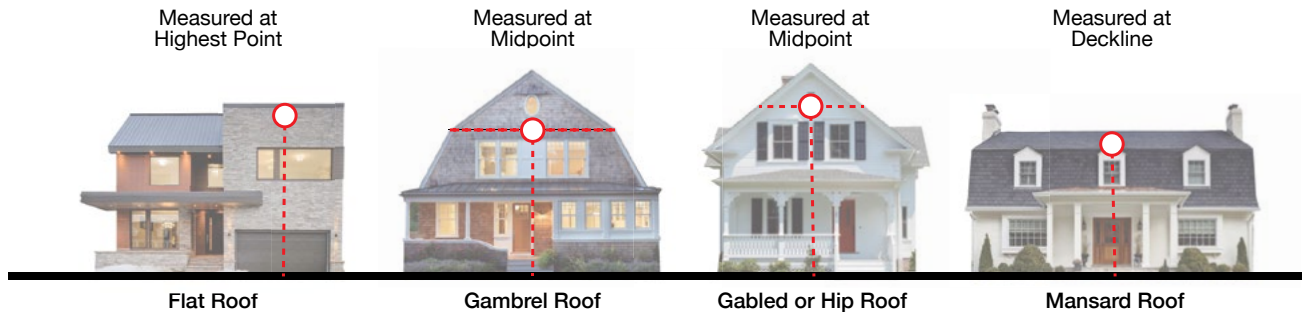


**Figure 4:** Examples of existing Built Form seen in Pickering's Neighbourhood Precincts.



## 2.1 Dwelling Height & Roof Pitch

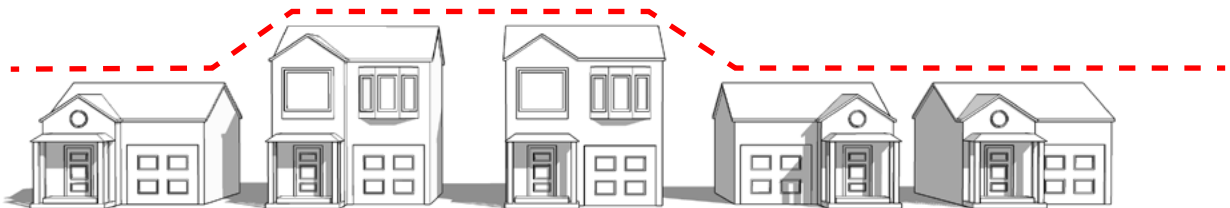
Dwelling height can be defined as the overall height of a dwelling, typically measured from the average established grade of the ground. The point of a dwelling to which height is measured varies depending on the type and pitch of roof that exists on a dwelling (Figure 5).



**Figure 5:** Examples of how building height is measured by the City of Pickering.

The following guidelines ensure a compatible building height and roof style along a street.

1. The height and roof pitch of a new home or addition should be compatible with the general scale and shapes of surrounding houses (Figure 6); and
2. For buildings with the highest point above 8.5 metres, the roof should be limited to two storeys and sloped back from adjacent houses.



**Figure 6:** Appropriate height transitions between buildings.

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## 2.2 Height of Front Entrance

The location of front entrances of dwellings along a street is a contributor to the character, comfort and safety of a neighbourhood. The height of the entrance ensures the front door of the dwelling is directly visible and easily accessible from the street. Figure 7 shows the preferred number of front steps for a front entrance.



**Figure 7:** Examples showing preferred, maximum, and prohibited front entrance heights.

The following guidelines have been developed to promote compatibility of dwelling scale as perceived from the street.

1. The height of the front entrance of a dwelling should be located at a height that is compatible with the height of front entrances of neighbouring dwellings, and provide for a maximum number of six (6) steps to access the front door (Figure 8);
2. The main entrance to the dwelling should be directly visible from the street;
3. The design and detailing of the main entrance should be consistent with the architectural style of the dwelling (Figure 9);



**Figure 8:** Front entrance with 6 steps or less.



**Figure 9:** Main entrance architecture is in keeping with the style of the dwelling.



4. Weather protection at the main entrance should be provided through the use of covered porches, porticos, canopies, verandas or recesses (Figure 10);
5. Natural light at the entry is encouraged though the use of sidelights, transoms and door glazing.
6. Enhancements to emphasize the main entry area is encouraged and may include pilasters and masonry surrounds;
7. Stairs accessing the main entrance to the dwelling should be designed as an integral component of the dwelling's façade;
8. Access routes should be provided for people with disabilities whenever possible;
9. The front entrance design and architectural elements should reduce the visual dominance of the garage and the front driveway (Figure 11); and
10. Subject to site grading conditions, additional sets of steps, separated by a pathway, may be permitted in order to promote a more comfortable pedestrian experience (Figure 12).



**Figure 10:** Front entrance should have weather protection.



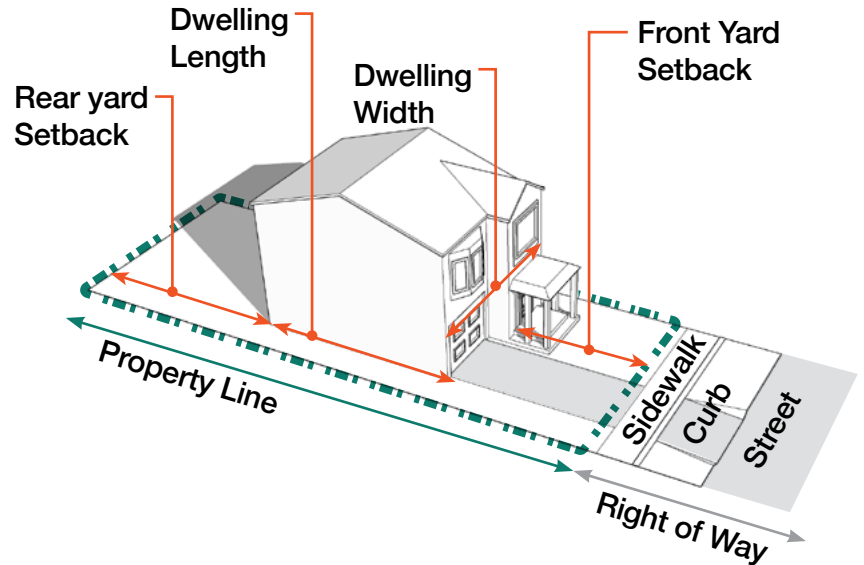
**Figure 11:** Front entrance design that reduces the visual dominance of the garage.



**Figure 12:** Additional steps may be permitted due to grade changes.

## 2.3 Dwelling Length, Width & Depth

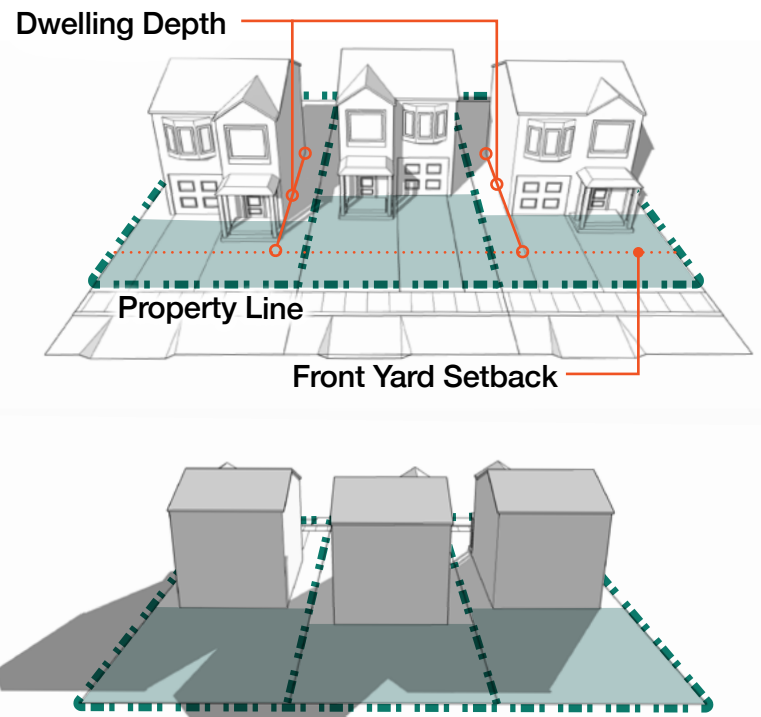
Dwelling length is the distance measured between the front and rear main walls of a dwelling, and Dwelling width is the distance measured between both side main walls of a dwelling (Figure 13). Dwelling depth is the measurement of the distance between the minimum front yard setback and the rear of the dwelling, which is a measure of how deep a dwelling protrudes into a lot, irrespective of the actual length of that same dwelling (Figure 14).



**Figure 13:** Example of dwelling length, and width.

The following guidelines help guide appropriate dwelling depths, lengths and widths that are compatible with other dwellings on the same street (Figure 13 & 14):

1. For new dwellings or dwelling additions, Dwelling Length and Width should be in keeping with the rhythm of the street.
2. Dwelling depth should be generally in keeping with the existing dwellings along a street to avoid privacy and overshadow issues.



**Figure 14:** Example of similar front and rear yard setbacks, and dwelling depths.





**You can't really say what is beautiful about a place, but the image of the place will remain vividly with you.**



**Tadao Ando**

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# 3

## Streetscape

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Streetscape examines the relationship of buildings to the street and other buildings, as well as the other defining landscape characteristics of properties within the private realm.

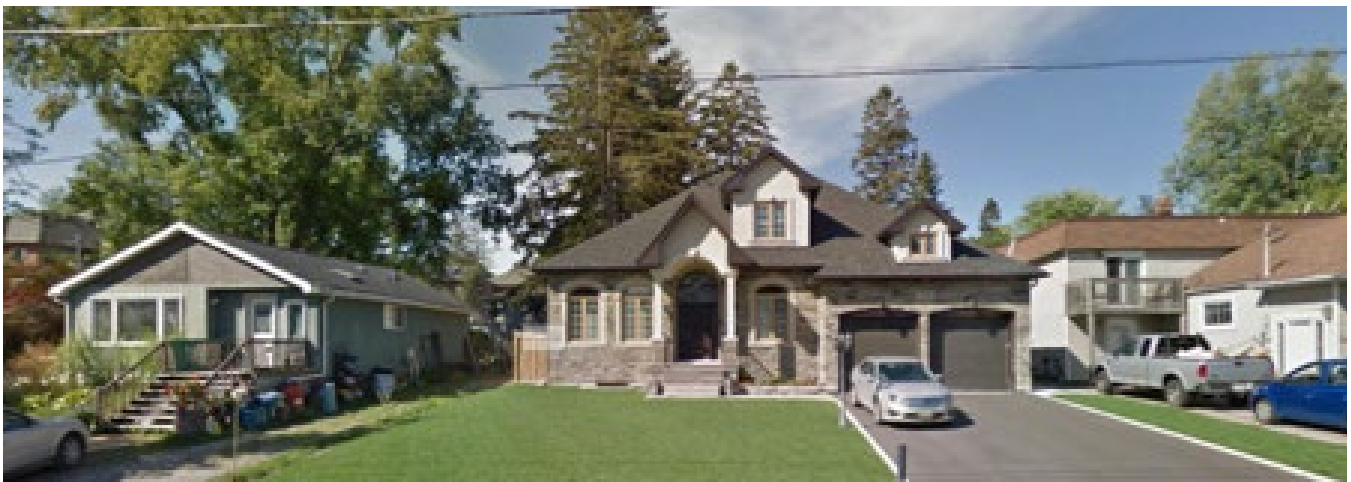
The following summarizes the compatibility issues that are currently associated with the key elements of Streetscape in Pickering's Neighbourhood Precincts related to:

- Side yard setback and separation distance between dwellings;
- Garage or carport placement;
- Driveway width; and
- Front Yard Landscaping (see Section 4.1 for more details).

It is common for larger replacement dwellings to be built much closer to the side property line than the original homes while still complying with the required zone standards. This affects the established consistency of larger separation distances between dwellings. The result changes the pattern of development and creates a sense of crowdedness along the street.

The garages and/or carports of original houses are typically sized for one vehicle and are sometimes set back from the front façade and therefore do not dominate the front of the dwelling. New infill and replacement houses typically have wider garages that are integral to the house to accommodate two or more vehicles.

New infill and replacement dwellings often have wider driveways than those of the original dwellings within an established neighbourhood, which reduce the amount of landscaping on the property and alter the open space character of a lot and streetscape.





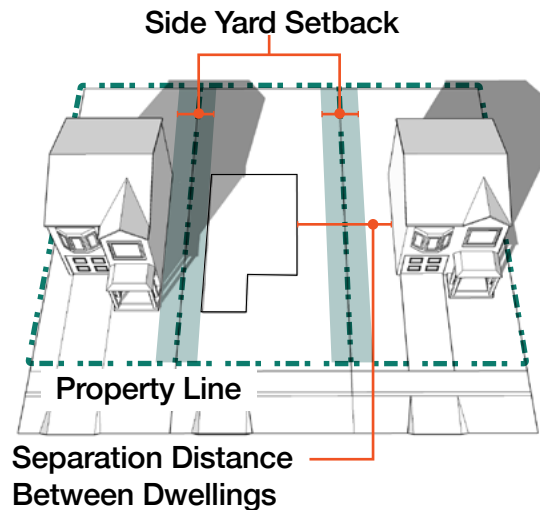
### 3.1 Side Yard Setback & Separation Distance Between Dwellings

Side Yard Setback refers to the distance between the side property line and the nearest exterior side wall of the dwelling on that property. Separation Distance Between Dwellings refers to the distance between the side wall of one dwelling to the nearest side wall of the adjacent dwelling (Figure 15).

The Separation Distance Between Dwellings, whether large or small, has an impact on the perceived sense of open space in a neighbourhood. Typically, in Neighbourhood Precincts in Pickering, there is a larger separation distance between the original dwellings in that neighbourhood when compared to newer subdivisions in other parts of the city. A larger separation distance between dwellings has the effect of reducing the perceived massing of a dwelling and increasing the opportunity for providing landscaped open space.

The following guidelines for Side Yard Setback and separation distance have been developed to reinforce the open space patterns within Neighbourhood Precincts.

1. Adequate Separation Distance Between Dwellings should be maintained to reinforce open space patterns between dwellings on the same block (Figure 16); and
2. Where needed, greater Side Yard Setbacks should be used to mitigate shadowing associated with greater building mass.



**Figure 15:** Examples of separation distance between dwellings and side yard setback.



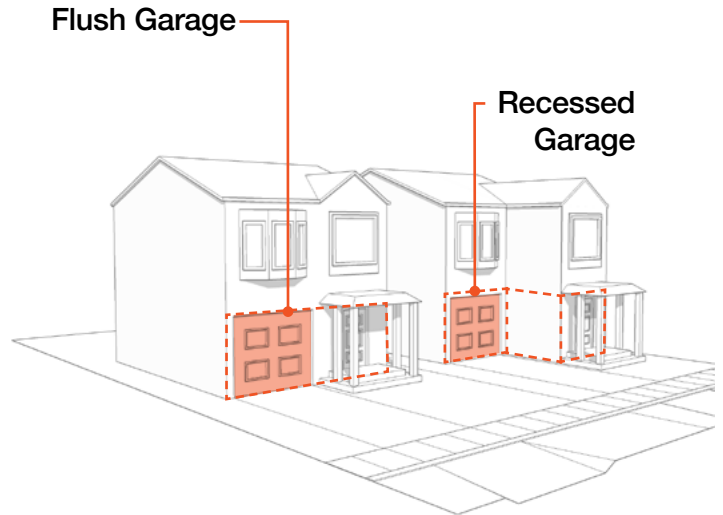
**Figure 16:** Google map aerial of Cliffview Road and Park Crescent showing the replacement housing (yellow) differs in side yard setback and separation distance between houses as compared to original dwellings (red).

## 3.2 Garage or Carport Placement

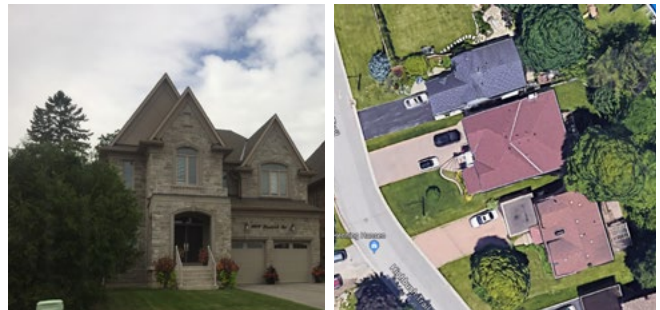
The location and placement of a garage or carport has an impact on the streetscape and overall character of a neighbourhood. In Pickering, Neighbourhood Precincts have an eclectic character with respect to garage and carport placement.

The following garage or carport placement guidelines have been developed to minimize the impact of projecting garages, where they are part of the neighbourhood character

1. Garage and parking configurations should minimize the appearance of garage doors from the street (Figure 17);
2. To minimize the massing impacts of projecting garages they should have sloped roofs;
3. Double car garages should have two single garage doors separated by a masonry column or, in the case of a full double wide door garage, a style with the appearance of two single bay doors, and a centre pier should be encouraged (Figure 18);
4. Projecting garages are only permitted in neighbourhoods where there are multiple pre-existing projecting garages on both sides of the street (Figure 19); and
5. In neighbourhoods where projecting garages are not part of the neighbourhood character, all new infill and replacement dwellings are to have the garages either flush with, or recessed behind, the front main wall of a dwelling.



**Figure 17:** Examples of garage and carport placement in relation to the main front wall of the dwelling.



**Figure 18:** Garages recessed behind the front entrance minimize the appearance of garage doors.

**Figure 19:** Examples of multiple projecting garages.



### 3.3 Driveway Width

The width of a driveway can have a significant impact on the streetscape of a neighbourhood. New infill and replacement dwellings with wider driveways to accommodate a greater number of vehicles, when compared to prevalent driveway widths, have the effect of reducing the amount of landscaping on a property, removing mature trees, limiting the space for street trees and impacting the character of the street. Driveway widths also have an impact on infiltration into the soil.

The following guidelines have been developed to minimize driveway width and help minimize the impact to the streetscape, to support the character of the Neighbourhood Precincts.

1. Driveways are to be no wider than the width of the permitted garage (Figure 20);
2. To maximize water infiltration, consideration should be given to the use of permeable pavers and other technologies (Figure 21); and
3. To ensure that there is sufficient space for planting street trees between driveways within the public right of way, tapering driveway widths to 6 metres is encouraged where appropriate.



**Figure 20:** The driveway width is no wider than the garage.



**Figure 21:** Example of a permeable paver system appropriate for driveways and pathways.



**Cities have the capability  
of providing something for  
everybody, only because,  
and only when, they are  
created by everybody.**



**Jane Jacobs**



# 4 Neighbourhood Composition

Neighbourhood Composition generally includes elements in the public realm that contribute to neighbourhood character. In the case of the City’s established neighbourhoods, private landscaping in front yards including mature trees, and street trees, contribute to the overall character of the neighbourhoods. In order to help maintain and enhance the character of the public realm, landscaping, preservation of existing trees, and tree planting is encouraged.

## 4.1 Front Yard Landscaping

Front yards provide an opportunity to add to the neighbourhood composition by creating a great streetscape. As well, landscaping and trees play an important role in lowering summer temperatures and providing areas for rainwater to filter into the ground.

The following guidelines have been developed to ensure that front yard landscaping enhances the character of the streetscape:

1. Existing mature trees should be preserved whenever possible (Figure 22).

2. Native tree species should be chosen that are hardy for the location and that are drought and salt tolerant, and disease resistant (Figure 22).
3. Avoid landscaping that completely screens the view of the house from the sidewalk/ street.
4. Use ground cover or other low-growing plants to maintain visibility of the house.
5. Use drought-resistant native plant species to avoid having to continuously water. (Figure 23)
6. Limit paving in the front yard to walkways and small areas at the front door.



**Figure 22:** Preserve mature trees wherever possible.



**Figure 23:** sustainable and resilient front yard landscaping.

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## 4.2 Street Trees

Planting street trees as part of an infill or replacement project goes a long way to add to the character of the neighbourhood. The following guidelines have been prepared to ensure that street trees are provided as part of the infill or replacement dwelling:

1. Trees on all streets should be a diverse mixture of species to limit the ability for diseases to spread. If a uniform look is desired it can be achieved by focusing on matching the form of trees rather than using the same species (Figure 24).
2. Selection of trees should take into consideration mature tree size, proximity to power lines and setback to existing trees or structures.
3. Wherever possible large canopy shade trees should be planted adjacent to sidewalks to reduce the heat island effect and enhance pedestrian comfort and safety.
4. Adequate space should be provided for trunk expansion.
5. Native tree species should be chosen that are hardy for the location and that are drought and salt tolerant, and disease resistant (Figure 25).
6. Street trees should be spaced at 10 to 12 metres apart or seek to reflect the existing placement pattern of street trees (Figure 26).



**Figure 24:** A mixture of tree species with the same form along a street add to the character and resiliency.



**Figure 25:** Several varieties of Maple Trees are native and provide fall colour.



**Figure 26:** Street Trees should be planted 10 to 12 metres apart.

# Appendix A

## Urban Design Guideline Checklist

### City of Pickering Established Neighbourhood Precincts Urban Design Checklist

Please note, if you mark “no” below please provide your rationale in the adjacent “Comments” section either supporting, or not supporting the proposal.

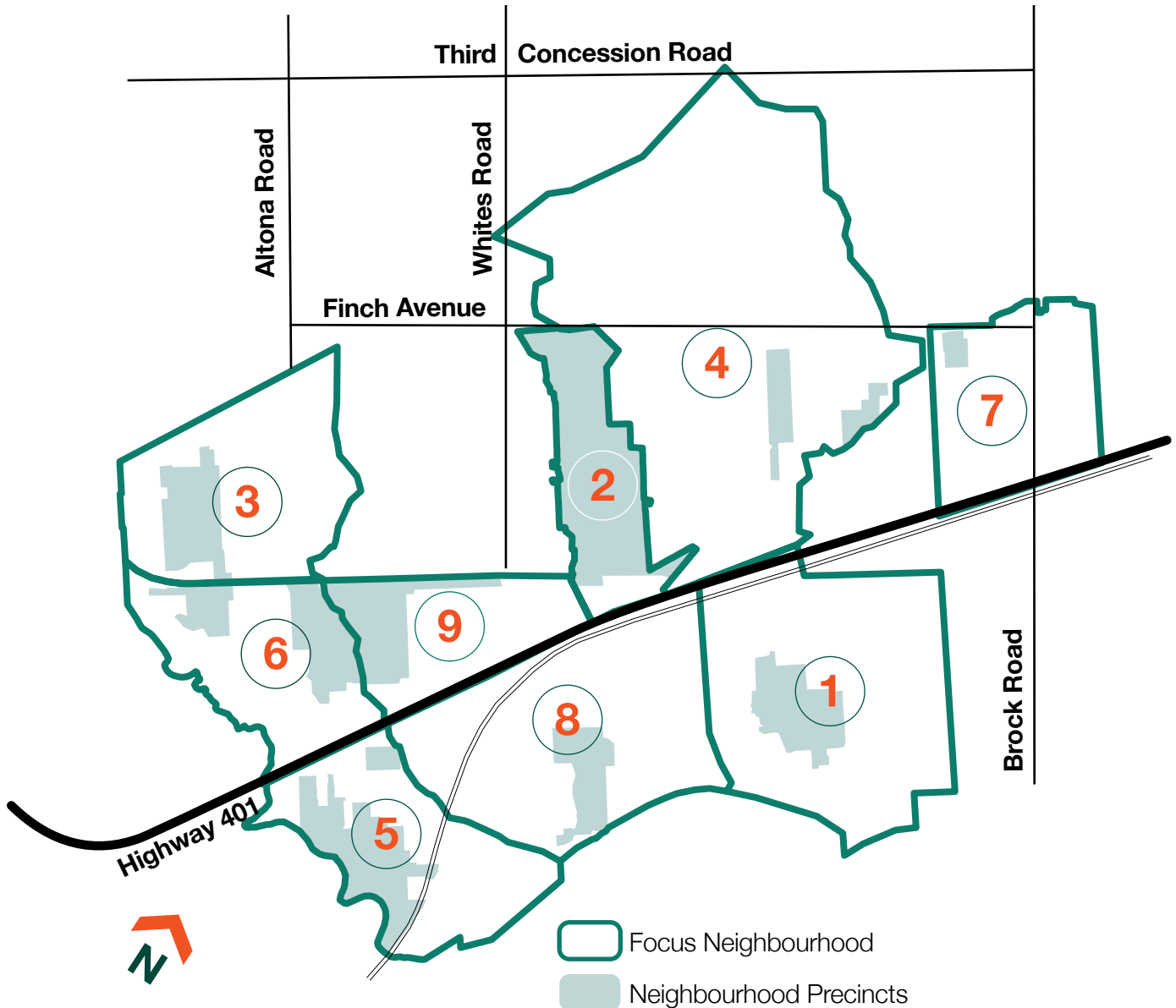
YES	NO		Comments
<input type="radio"/>	<input type="radio"/>	1. Does the proposed dwelling have a sloped roof proposed such as a Hip, Gable, Mansard or Gambrel? (see Figure 5)	
<input type="radio"/>	<input type="radio"/>	2. Is the proposed dwelling height and roof pitch similar/compatible with the surrounding dwellings? (see Section 2.1: Guideline 1)	
<input type="radio"/>	<input type="radio"/>	3. For dwellings with a height greater than 8.5 metres - is the dwelling a maximum two storeys with a sloped roof back from the adjacent dwellings? (see Section 2.1: Guideline 2)	
<input type="radio"/>	<input type="radio"/>	4. Does the front entrance have 6 or less steps? (see Section 2.2: Guideline 1)	
<input type="radio"/>	<input type="radio"/>	5. Is the main entrance visible from the street? (see Section 2.2: Guideline 2)	
<input type="radio"/>	<input type="radio"/>	6. Is the design of the main entrance consistent with the architectural style of the dwelling? (Section 2.2: Guidelines 3 and 4)	
<input type="radio"/>	<input type="radio"/>	7. Does the main entrance include a porch, portico or other weather protection in keeping with the design of the dwelling? (see Section 2.2: Guideline 4)	
<input type="radio"/>	<input type="radio"/>	8. Are the stairs to the main entrance designed as an integral component of the front façade? (Section 2.2: Guideline 7)	
<input type="radio"/>	<input type="radio"/>	9. Does the design of the front entrance reduce the visual dominance of the garage and driveway? (Section 2.2: Guideline 9)	
<input type="radio"/>	<input type="radio"/>	10. Does the proposed dwelling have a similar Dwelling Depth to the adjacent dwellings along the street? (see Section 2.3: Guideline 2)	



YES	NO	Comments
<input type="radio"/>	<input type="radio"/>	<b>11.</b> Does the proposed dwelling have a similar Side Yard Setback to the adjacent dwellings along the street? (see Figure 15)
<input type="radio"/>	<input type="radio"/>	<b>12.</b> Has shadow on adjacent dwellings been mitigated with greater setbacks? (Section 3.1: Guideline 2)
<input type="radio"/>	<input type="radio"/>	<b>13.</b> If a projecting garage is permitted, does it have a sloped roof? (see Section 3.2: Guidelines 2 and 4)
<input type="radio"/>	<input type="radio"/>	<b>14.</b> If a double car garage is proposed, does it have 2 single doors or is it designed to look like 2 separate doors? (see Section 3.2: Guideline 3)
<input type="radio"/>	<input type="radio"/>	<b>15.</b> Is the garage flush or recessed from the main front wall? (see Section 3.2: Guideline 5)
<input type="radio"/>	<input type="radio"/>	<b>16.</b> Is the proposed driveway width the same as the permitted garage width? (see Section 3.3: Guideline 1)
<input type="radio"/>	<input type="radio"/>	<b>17.</b> Are sustainable design features or resilient landscaping proposed as part of the site design? (Section 3.3: Guideline 2 and Section 4.1: Guideline 5)
<input type="radio"/>	<input type="radio"/>	<b>18.</b> Does the plan preserve existing trees? (see Section 4.1: Guideline 1)
<input type="radio"/>	<input type="radio"/>	<b>19.</b> Does the plan include tree planting on private property? (see Section 4.1: Guideline 2)
<input type="radio"/>	<input type="radio"/>	<b>20.</b> Does the plan include one or more native species street trees? (Section 4.2)

# Appendix B

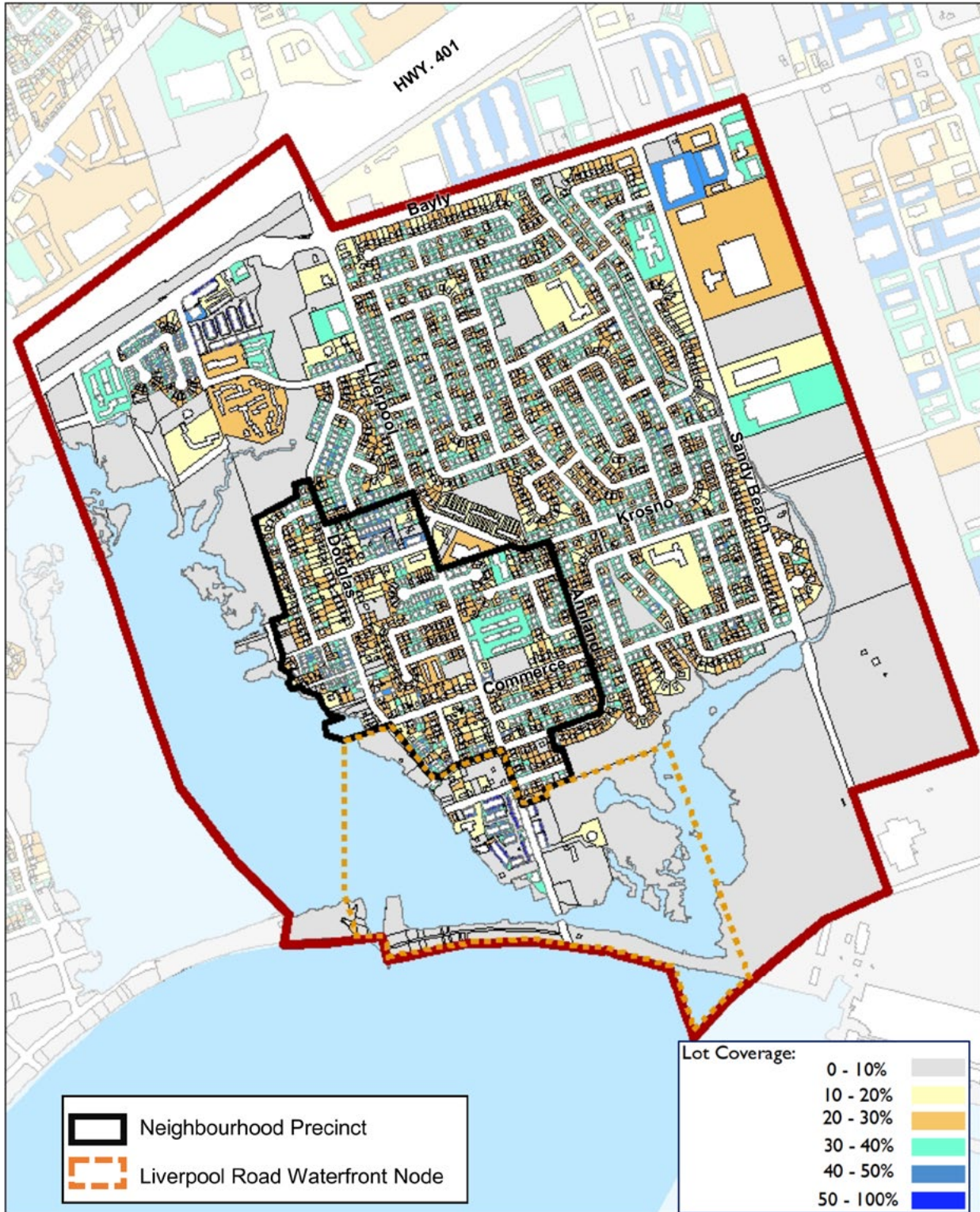
## Focus Neighbourhoods & Precinct Map



### FOCUS NEIGHBOURHOODS

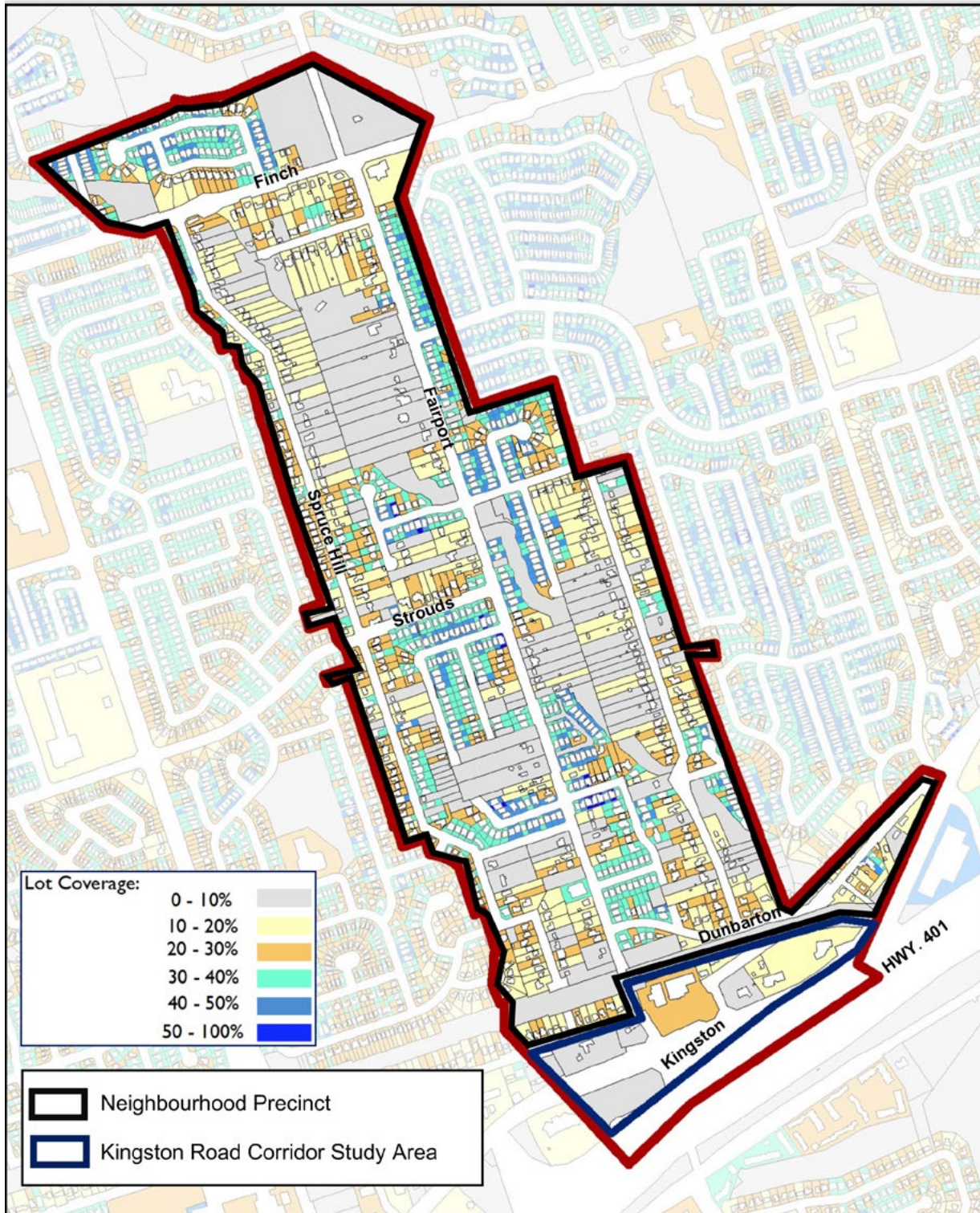
- |               |               |                 |
|---------------|---------------|-----------------|
| 1. Bay Ridges | 4. Liverpool  | 7. Village East |
| 2. Dunbarton  | 5. Rosebank   | 8. West Shore   |
| 3. Highbush   | 6. Rougemount | 9. Woodlands    |

# Bay Ridges



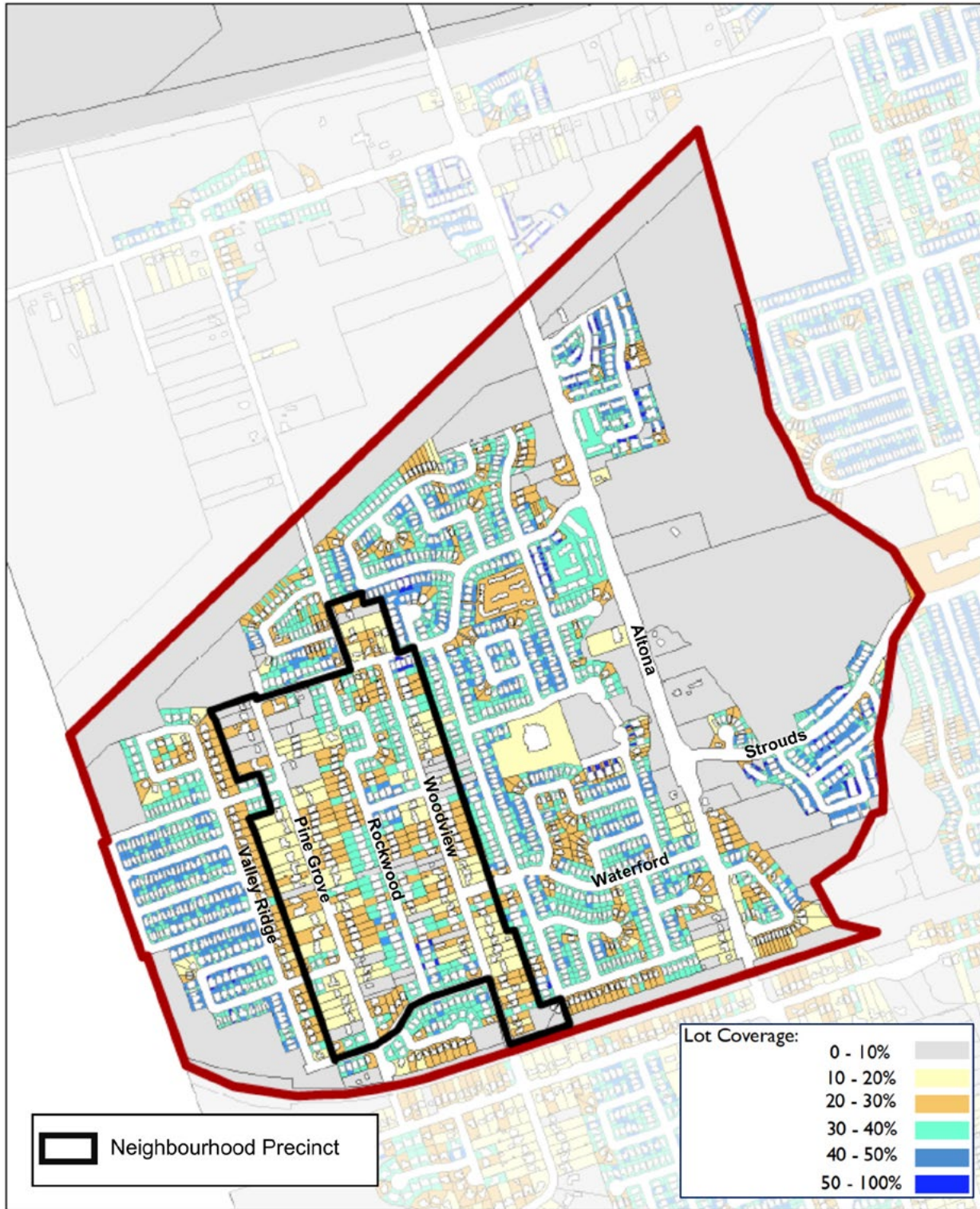


# Dunbarton





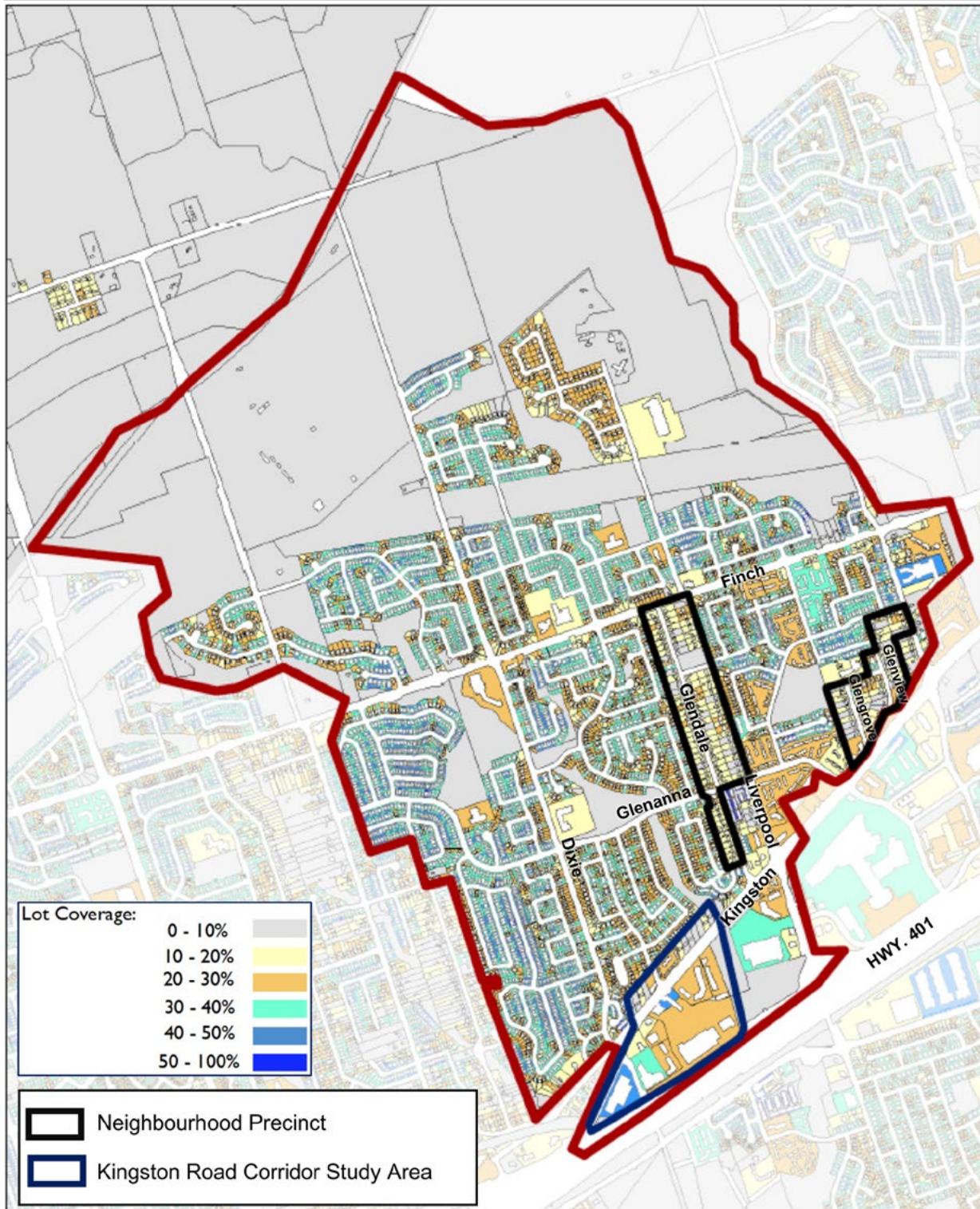
# Highbush



Neighbourhood Precinct

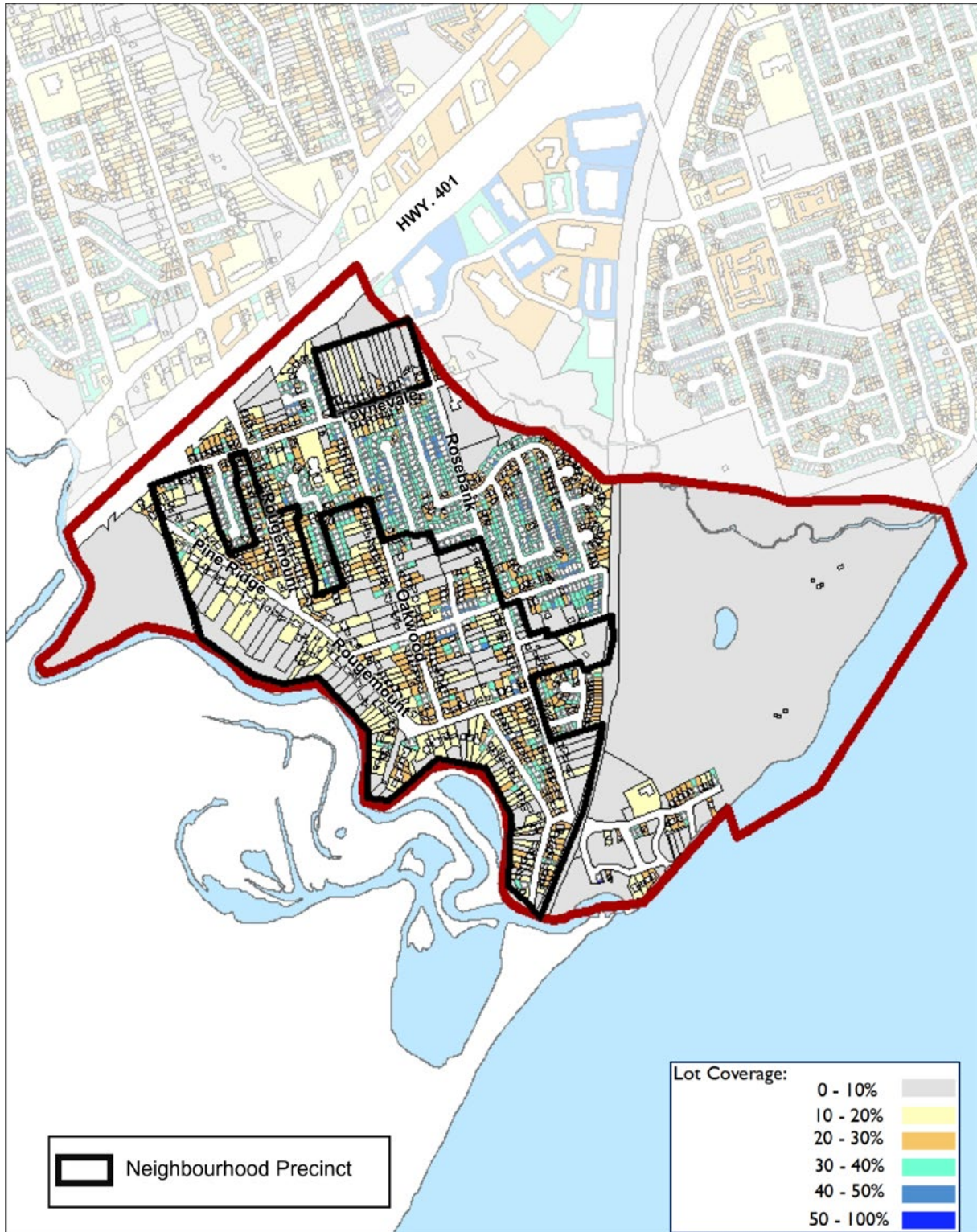


# Liverpool

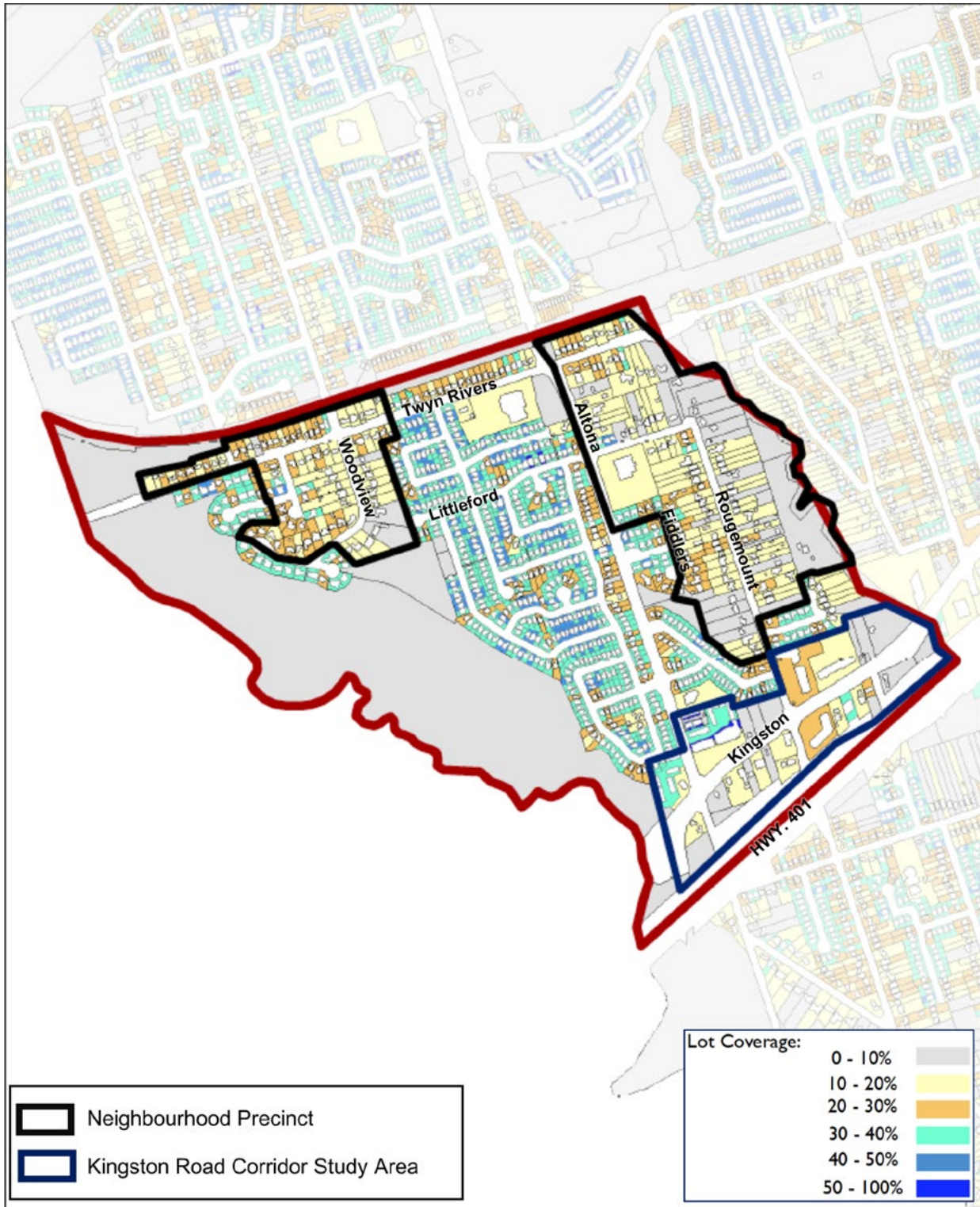




# Rosebank

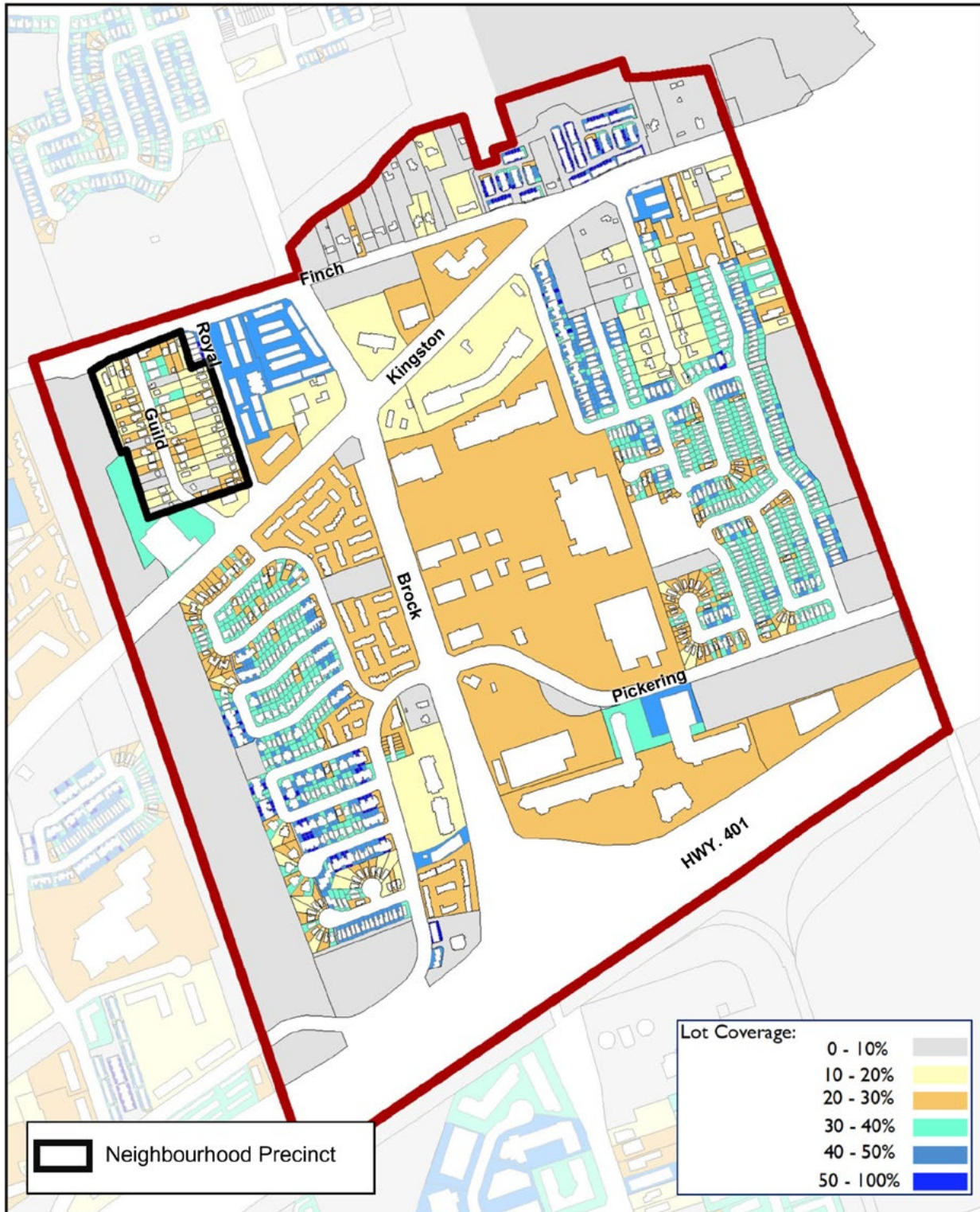


# Rougemount





# Village East





# West Shore





# Woodlands

